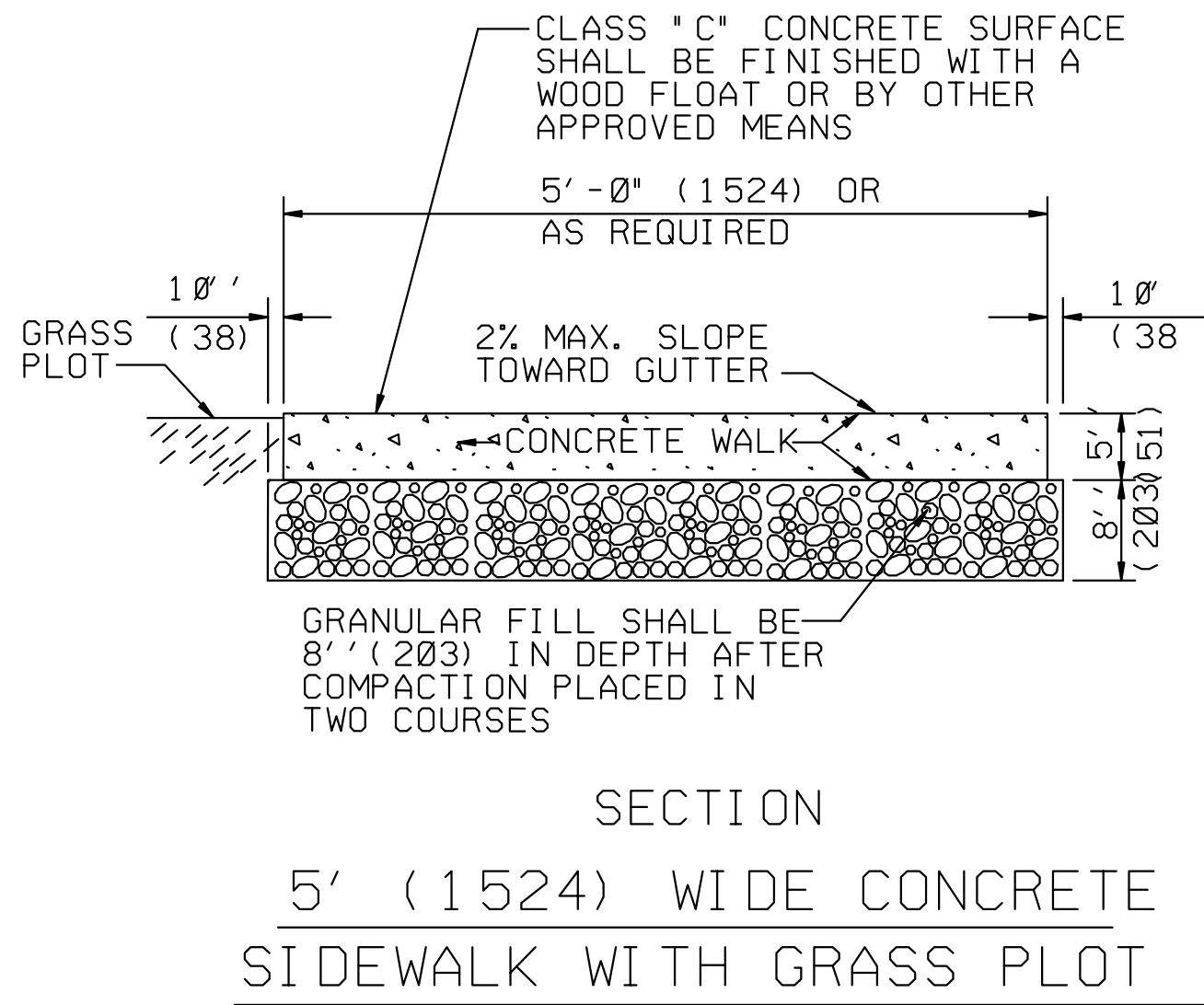


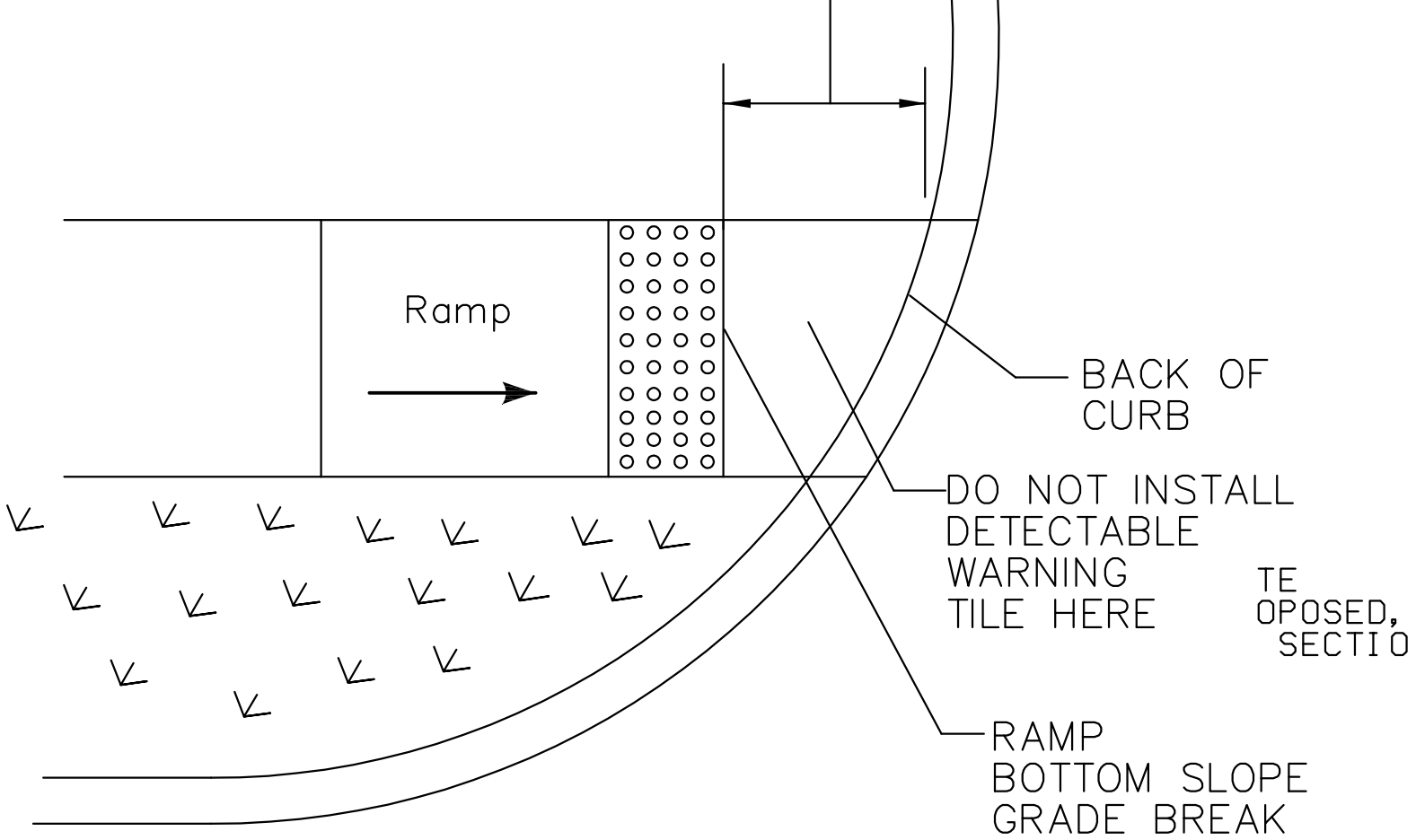
DATE	REVISION	CK.
03/09/15	PLANS REVISED PER APPROVAL	
04/08/15	PROPOSED CROSSWALK & SIGNAGE REVISED	
04/24/15	PROPOSED SIGNAGE REVISED	
06/10/15	SANITARIANS COMMENTS ADDRESSED	
06/18/15	RESPOND TO 06-18-15 SANITARIANS COMMENTS	
07/30/15	REVISE PER DPH COMMENTS	
08/01/15	PROPOSED TRANSFER & GENERATOR PAD LOCATIONS ADDED	
09/02/15	STATE HEALTH DEPARTMENT COMMENTS ADDRESSED	



GENERAL NOTES:

1. DRIVEWAY ENTRANCE SHALL BE A MINIMUM OF 12' (365) WIDE, EXCLUDING CURBING WHEN PRESENT.
2. SIDEWALK RAMPS SHALL BE A MINIMUM OF 36" (914) T (1016) MAXIMUM, WITH A MAXIMUM SLOPE OF 12:1. THE SHALL BE NO LIP AT THE DRIVEWAY SIDEWALK INTERFAC
3. WELDED WIRE FABRIC MATS WITH REINFORCING AT CLOSE SPACING MAY BE USED.

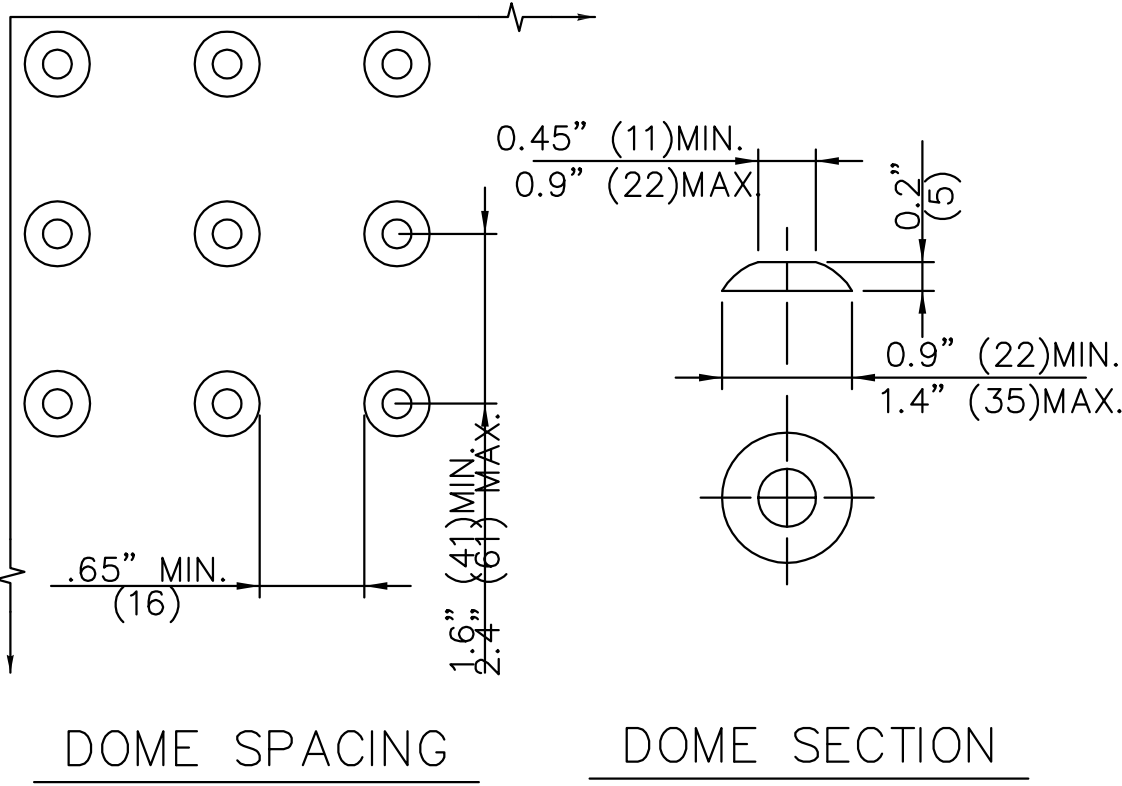
NOTE : IF BOTH ENDS OF THE RAMP
BOTTOM SLOPE GRADE BREAK
ARE LESS THAN 5' (1.5m) FROM
BACK OF CURB, PLACE DETECTABLE
WARNING TILE ON THE RAMP RUN
AT THE BOTTOM GRADE BREAK



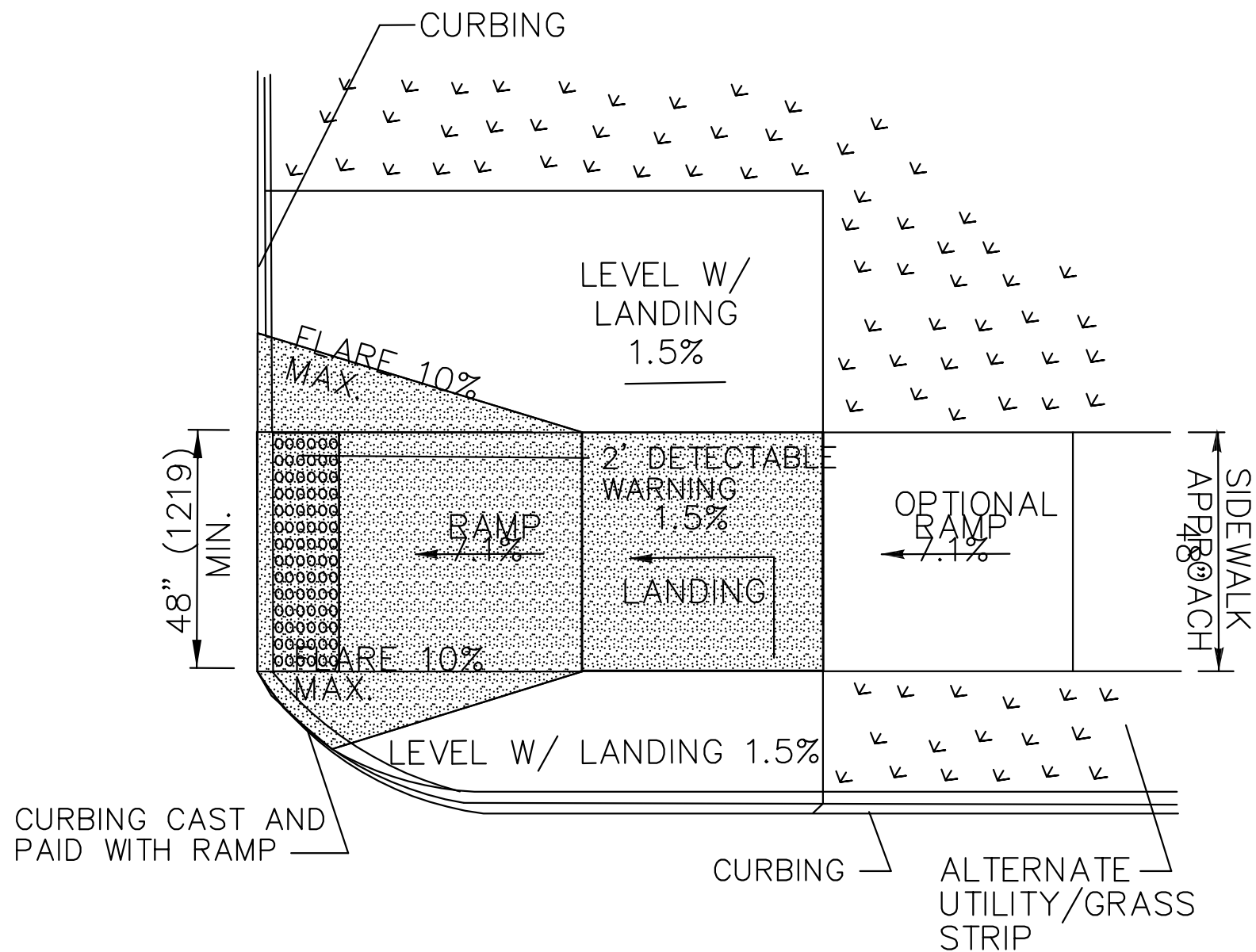
DETECTABLE WARNING
PLACEMENT DETAIL 2

GENERAL NOTES:

1. RAMPED MEDIANS SHALL HAVE A CURB RAMP AT EITHER END AND LEVEL LANDING A MINIMUM OF 5' x 5' (1.5m x 1.5m) IN BETWEEN. CUT-THROUGH MEDIANS SHALL BE A MINIMUM OF 6' (1.8m) LONG AND 5' (1.5m) WIDE. FOR ALL MEDIANS, CUT-THROUGH OR RAMPED, A 2' (610) STRIP OF DETECTABLE WARNINGS SHALL BE INSTALLED AT THE ENTRANCE AND EXIT.
2. SEE GENERAL NOTES ON SHEET 1.

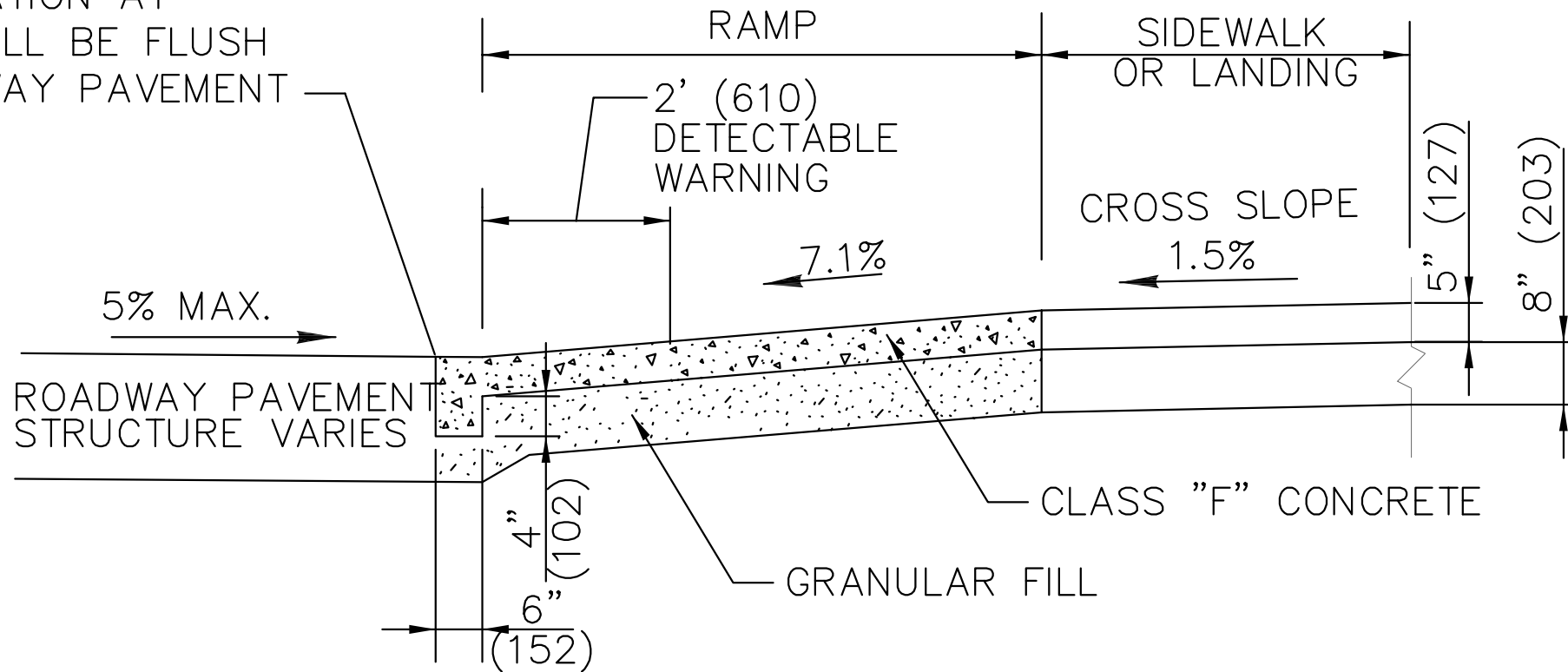


STANDARD DOME ON DETECTABLE WARNING TILES



PERPENDICULAR SIDEWALK RAMP
ONE DIRECTION ON CORNER
(TYPE 6a)

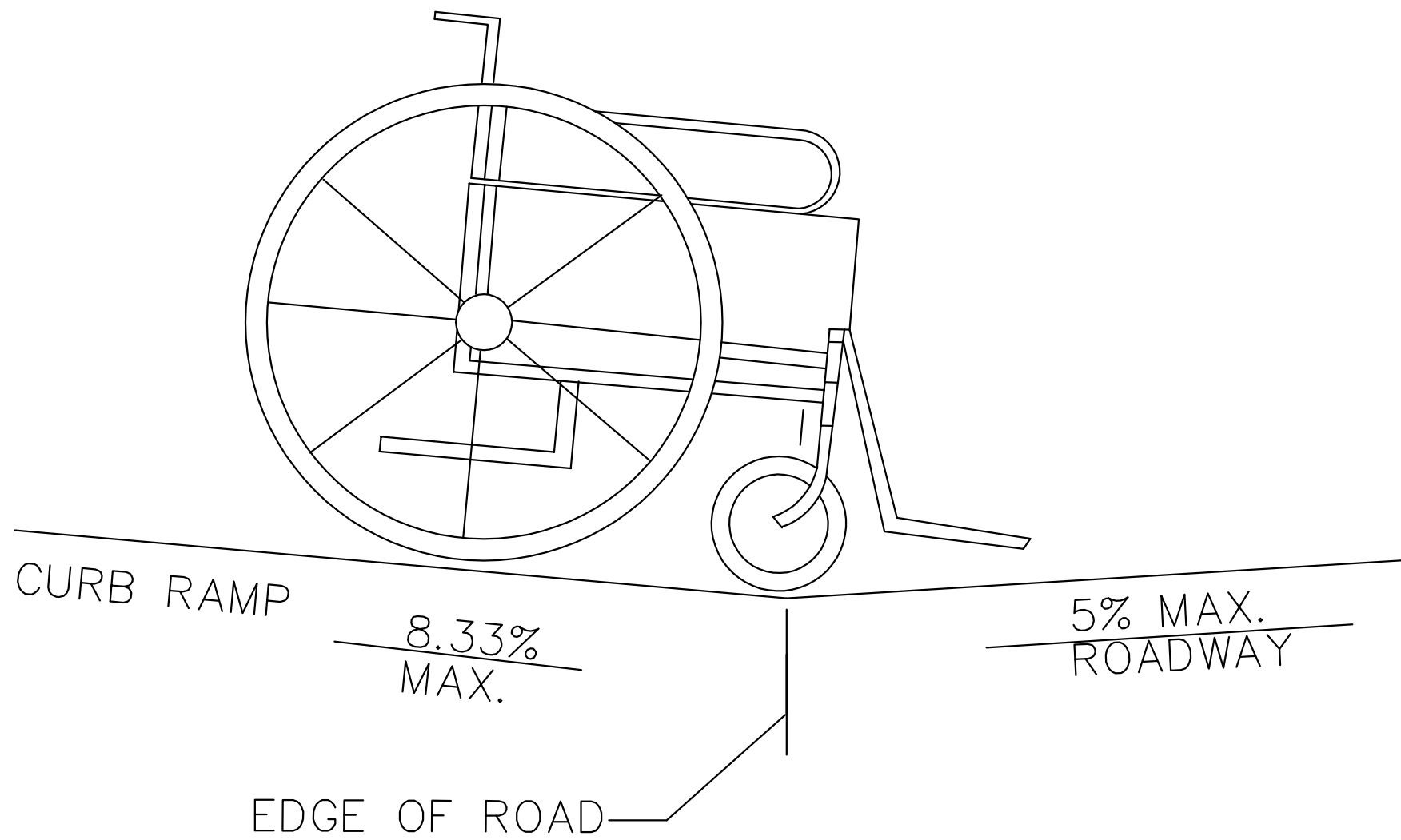
RAMP ELEVATION AT
GUTTER SHALL BE FLUSH
WITH ROADWAY PAVEMENT



TYPICAL SECTION THRU SIDEWALK RAMP
SEE NOTE 2 AND 17 SHEET 1

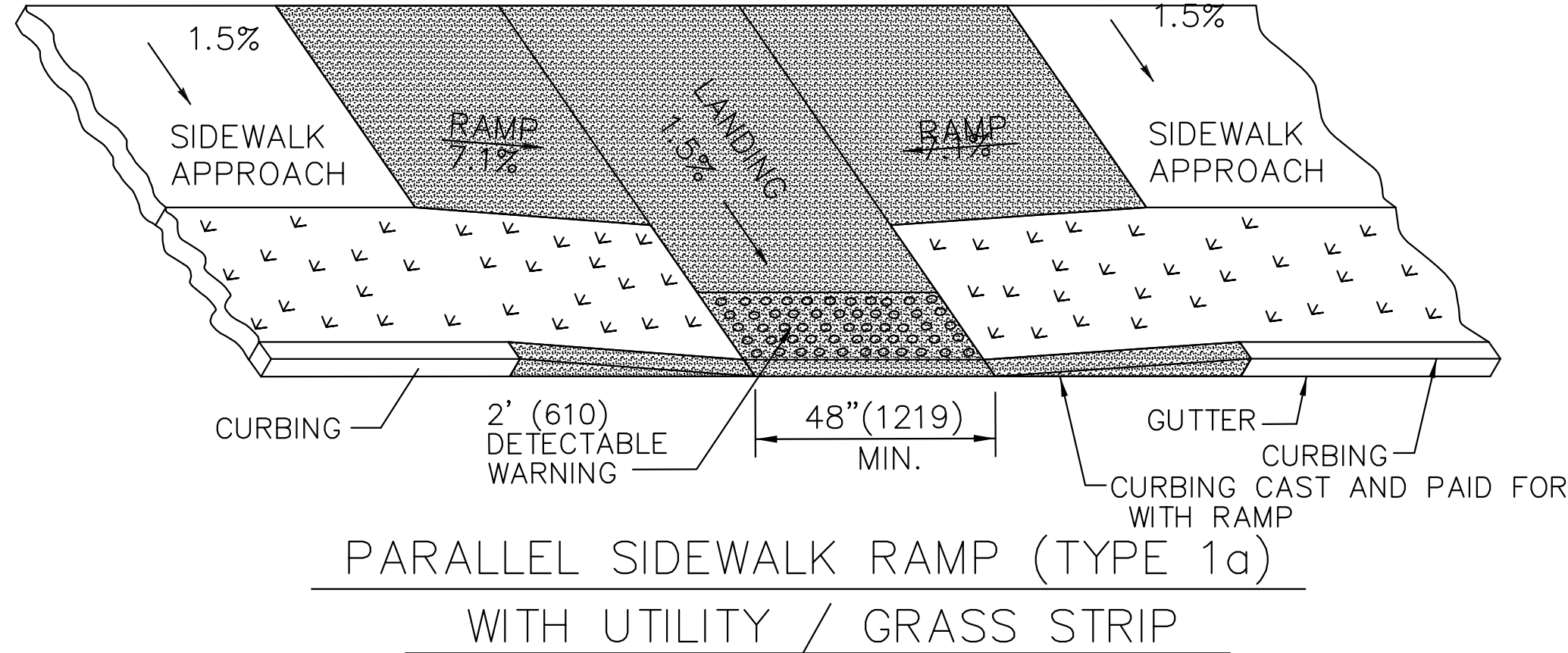
GENERAL NOTES:

1. MAXIMUM SLOPES OF ADJOINING GUTTERS AND ROAD SURFACES IMMEDIATELY ADJACENT TO THE SIDEWALK RAMP SHOULD NOT EXCEED 5%. THE MAXIMUM GRADE DIFFERENCE BETWEEN THE GUTTER AND CURB RAMP SHALL NOT EXCEED 13%. SEE DETAIL 1 ON SHEET 4.
2. RAMP GRADE SHALL BE UNIFORM, FREE OF SAGS AND ABRUPT GRADE CHANGES. RUNNING SLOPES OF RAMPS SHALL NOT EXCEED 8.33% AND SHALL NOT EXCEED 15' (4.5m) WITHOUT PROVIDING A LANDING.
3. ALL RAMPS SHALL BE CONSTRUCTED OF CLASS "F" CONCRETE IN ACCORDANCE WITH CONNECTICUT STANDARD SPECIFICATIONS.
4. SIDEWALK RAMPS SHALL HAVE A COARSE BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP. THE SURFACE OF ALL SIDEWALK RAMPS SHALL BE STABLE, FIRM AND SLIP RESISTANT. SURFACE DISCONTINUITIES SHALL NOT EXCEED 2" (51) MAX. VERTICAL DISCONTINUITIES BETWEEN 2" (51) AND 4" (102) MAX. SHALL BE BEVELED 1:2 MINIMUM APPLIED ACROSS THE ENTIRE LEVEL CHANGE.
5. DIAGONAL SIDEWALK RAMPS AT MARKED CROSSINGS SHALL BE WHOLLY CONTAINED WITHIN THE MARKINGS, EXCLUDING ANY FLARED SIDES. DIAGONAL AND PERPENDICULAR RAMPS SHALL HAVE THE RAMP CUT PERPENDICULAR TO THE TANGENT OF THE CURB RADIUS FOR THE DESIGNATED ACCESSIBLE ROUTE. BOTH LONGITUDINAL SIDES OF THE RAMP SHOULD BE THE SAME LENGTH. SKEWED RAMPS SHOULD BE AVOIDED. FLARES ARE NOT CONSIDERED PART OF PEDESTRIAN ACCESS ROUTE. DIAGONAL RAMPS SHOULD NOT BE INSTALLED WHERE CURB RADII IS LESS THAN 20'(6096).
6. REMOVAL OF EXISTING SIDEWALK FOR NEW RAMP INSTALLATIONS SHALL BE TO THE NEAREST EXPANSION OR CONTRACTION JOINT. 8.33% MAXIMUM SLOPE MAY NOT BE ACHIEVABLE DUE TO EXISTING SIDEWALK GRADE. IN RECOGNITION OF THIS, A LIMIT OF 15' (4572) FOR REMOVAL SHALL BE USED UNLESS OTHERWISE SHOWN ON THE PLANS OR DIRECTED BY THE ENGINEER. SAW CUT REQUIRED FOR DUMMY JOINTS SHALL BE INCLUDED IN THE COST OF "CONCRETE SIDEWALK RAMP" OR "CONCRETE SIDEWALK".
7. EXPANSION JOINTS IN CONCRETE SHALL MATCH THOSE IN ADJACENT SIDEWALKS BUT IN NO CASE SHALL THE SPACING BETWEEN EXPANSION JOINTS EXCEED 12' (3658) UNLESS OTHERWISE NOTED.
8. CONCRETE SIDEWALK RAMPS, SHALL BE PAID FOR UNDER THE ITEM "CONCRETE SIDEWALK RAMP", AS DEFINED BY THE CONSTRUCTION LIMITS ON THE PLANS AND SHALL BE FIELD VERIFIED.
9. SIDEWALK RAMPS SHALL BE CONSTRUCTED WITH THE TOE AT THE GUTTER CAST INTEGRALLY WITH RAMP UNLESS DIRECTED OTHERWISE BY THE ENGINEER (SEE TYPICAL SECTION ON SHEET 3). CURB REMOVAL AND CAST IN PLACE CURBING REQUIRED FOR THE RAMP SHALL BE INCLUDED WITH PAY ITEM "CONCRETE SIDEWALK RAMP". CURBING OUTSIDE LIMITS OF RAMP OR LANDING SHOWN ON SHEET 3 SHALL BE CONSTRUCTED AND PAID FOR IN ACCORDANCE WITH CONNECTICUT STANDARD SPECIFICATIONS.
10. PREFERRED LOCATION TO INSTALL DETECTABLE WARNING STRIP SHALL BE 6" (152) FROM THE EDGE OF ROAD ALONG THE FULL WIDTH OF THE RAMP. FOR ALTERNATE LOCATIONS, REFER TO DETECTABLE WARNING PLACEMENT DETAILS ON SHEET 4.
11. TO PERMIT WHEELCHAIR WHEELS TO ROLL BETWEEN DOMES, ALIGN DOMES ON A SQUARE GRID IN THE DIRECTION OF RUNNING SLOPE (PERPENDICULAR TO CURB OR SLOPE BREAK). THE TRANSITION FROM RAMP TO GUTTER SHALL BE FLUSH WITHOUT A LIP.
12. WHERE COMMERCIAL DRIVEWAYS ARE PROVIDED WITH TRAFFIC SIGNALS AND THE SIDEWALK IS CONTINUOUS THROUGH DRIVEWAY, DETECTABLE WARNINGS ARE REQUIRED AT THE JUNCTION BETWEEN THE PEDESTRIAN ROUTE AND DRIVEWAY.
13. CONSTRUCT A SIDEWALK CURB WHEN THERE IS INSUFFICIENT BUFFER AVAILABLE TO GRADE OR WHEN CALLED FOR IN PLANS. PAID FOR WITH SIDEWALK RAMP WHEN REQUIRED FOR RAMP.
14. THE TOP AND BOTTOM OF RAMPS SHOULD BE PROVIDED WITH A 4' x 4' (1219 x 1219) MINIMUM LEVEL LANDING AREA WITH A CROSS SLOPE LESS THAN OR EQUAL TO 2% IN ANY DIRECTION.
15. UTILITY POLES, LUMINAIRE, PEDESTRIAN OR SIGNAL POLES, GRATES, ACCESS COVERS, AND OTHER APPURTENANCES SHALL NOT BE LOCATED ON RAMPS, LANDINGS, BLENDED TRANSITIONS, AND @ GUTTERS WITHIN THE PEDESTRIAN ACCESS ROUTE.
16. APPROACH SIDEWALK WIDTHS, GRASS STRIP OR UTILITY STRIP WIDTHS MAY VARY.
17. APPROACH SIDEWALK AND LANDING CROSS SLOPE SHALL NOT EXCEED 2%.
18. THE RUNNING OR CROSS SLOPES ON LANDINGS AT MID BLOCK CROSSING MAY BE WARPED TO MEET STREET OR HIGHWAY GRADE.
19. FOR PERPENDICULAR CURB RAMPS A MIN. 4'(1.2m) x 4'(1.2m) LEVEL LANDING SHALL BE PROVIDED AT THE TOP OF CURB RAMP WHERE THE LEVEL LANDING IS RESTRICTED AT THE BACK OF SIDEWALK THE LEVEL LANDING SHALL BE 4'(1.2m) x 5'(1.5m) WITH THE 5'(1.5m) DIMENSION PROVIDED IN THE DIRECTION OF THE RAMP RUN.
20. FOR PARALLEL CURB RAMPS, A MIN. 4'(1.2m) x 4'(1.2m) LEVEL LANDING SHALL BE PROVIDED AT THE BOTTOM OF CURB RAMP. IF THE LEVEL LANDING IS RESTRICTED ON 2 OR MORE SIDES, THE LEVEL LANDING SHALL BE 4'(1.2m) x 5'(1.5m) WITH THE 5'(1.5m) DIMENSION PROVIDED IN THE DIRECTION OF THE PEDESTRIAN STREET CROSSING.
21. WHEN WIDTH OF SIDEWALK IS >48" AND A PERPENDICULAR SIDEWALK RAMP IS INSTALLED, THE FLARED SIDES SHALL BE 10% MAX. IF WIDTH OF SIDEWALK IS <48" THE FLARED SIDES MUST NOT EXCEED 8.33% (12:1).
22. SHADED AREAS ARE TYPICAL PAY LIMITS FOR CONCRETE SIDEWALK RAMP BUT, MAY VARY AS DIRECTED BY THE ENGINEER.
23. OPTIONAL RAMP, WHEN REQUIRED, SHALL BE PAID FOR AS PART OF CONCRETE SIDEWALK RAMP.



DETAIL 1
SEE GRADE CHANGE AT ROADWAY INTERFACE

SEE NOTE 1 SHEET HW-921_02a

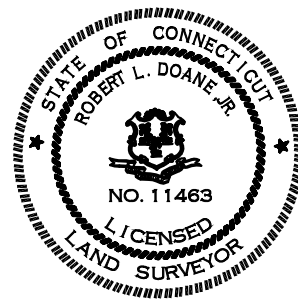
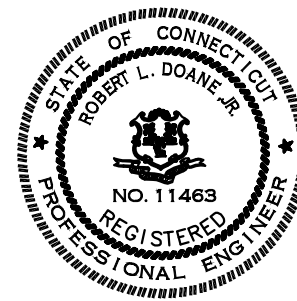


PARALLEL SIDEWALK RAMP (TYPE 1a)
WITH UTILITY / GRASS STRIP

TO MY KNOWLEDGE AND BELIEF THIS MAP IS SUBSTANTIALLY
CORRECT AS NOTED HEREON.

Robert L. Doane, Jr.

ROBERT L. DOANE, JR.
CONN. P.E. & L.S. LIC. NO. 11463



DETAILS & NOTES
MAP 180 LOT 113 & MAP 180 LOT 174

DOANE-COLLINS ENGINEERING ASSOCIATES, LLC
CIVIL ENGINEERING & LAND SURVEYING
P.O. BOX 113 CENTERBROOK, CT. 06409 (860)787-0138

SITE PLAN
SETTLERS LANDING
MULTI-FAMILY HOUSING FACILITY
PREPARED FOR
1961 BOSTON POST ROAD, LLC & VISTA VOCATIONAL & LIFE SKILLS CENTER, INC.
CHAPMAN BEACH ROAD, WESTBROOK, CT.

APPROVED BY THE TOWN OF WESTBROOK ZONING COMMISSION

ZONING CHAIRMAN _____ DATE _____

SCALE: AS SHOWN DATE: 02/22/14 SHEET NO: 5 OF 6 IDENT. NO: BDC-5 OF 7